Maine Department of Transportation Shoulder Surface Type Policy

Shoulder surface type will be based on the following policy. Projects currently programmed through current or past Biennial Transportation Improvement Programs (BTIPs) with gravel shoulders, will usually remain as programmed, as funds are not available to add shoulder paving. Therefore, with pavement preservation projects, this policy will fully become effective with the 2005-2005 BTIP.

A.) On the National Highway System - All shoulders shall be paved as part of new construction, highway improvement, or pavement preservation projects.

B.) Off the National Highway System -

1.) Where shoulders are warranted, as determined by state standards on new construction or highway improvements, the shoulders will be paved. Composite shoulders may be used for context sensitive design reasons. When the proposed pavement width of a composite shoulder is less than four feet, the lead unit will notify the Bicycle Coordinator.

2.) Pavement preservation projects

- a.) Existing paved shoulders will be resurfaced.
- b.) Pavement preservation projects will have gravel shoulders paved where the design year Summer Average Daily Traffic (SADT) is greater than 4000.
- c.) Pavement preservation projects where the design SADT is less than 4000, shoulders will be paved if the following applies; otherwise, shoulders will be gravel. If there is any uncertainty as to whether the criteria listed apply in any given situation, the lead unit will contact the Bicycle Coordinator.
 - i.) A bicycle route that is designated in the Department's bicycle plan.
 - ii.) On recreational use highways. These highways are defined by individual traffic counters or included in group lll recreational highways.
 - iii.) In villages, or adjacent to parks, schools, beaches, fairgrounds, recreation facilities, work centers, or other "built-up" areas to accommodate pedestrian and bicycle usage. This may include extending paved shoulders to a facility adjacent to the village.

- iv.) Complete gaps on highway segments where the majority of shoulders are already paved.
- v.) All guardrail locations.
- vi.) Between short gaps of guardrail runs and closely spaced side roads.
- vii.) Areas adjacent to side roads with many turning movements.
- C.) Hot Maintenance Mulch regardless of system
 - 1.) Existing gravel shoulders will remain gravel.
 - 2.) Existing paved shoulders will be resurfaced during Not Maintenance Mulch (HMM) paving when one of the following conditions apply. Otherwise, existing paved shoulders will not be resurfaced.
 - a.) A lip, 16 mm or greater, exists at the edge of the traveled way as a result of previous resurfacing of the traveled way and not resurfacing the shoulder.
 - b.) The existing paved shoulder is in such poor condition that it has an adverse impact on shoulder maintenance and use by pedestrians and bicyclists.
 - 3.) Shoulders being resurfaced under HMM contracts should not require extensive "hand work" which is outside the scope of work for HMM. An example of extensive "hand work" includes paved "spoon ditches".
 - 4.) If a municipality desires resurfaced shoulders, which in the opinion of the Department do not meet the above criteria, the shoulder resurfacing can be completed with the municipality responsible for the cost.
 - 5.) When shoulders are resurfaced, painting of pavement markings (edge line, etc.) is the responsibility of the entity who normally maintains the pavement markings.

D.) Glossary

Composite Shoulder - Multiple surface types across the width of a shoulder. Highway Improvement - Major rehabilitation or reconstruction of a roadway.

State Standards Highway Design Quide for Non-NHS Highways July 2003

Hot Maintenance Mulch - A pavement treatment used as a holding action until another level of treatment can be affected.

National Highway System (NHS) - A highway system, consisting primarily of existing Interstate routes and a portion of the federally designated principal arterial highways. These roads are considered most important to Interstate travel and national defense, they connect with other modes of transportation, and are essential for international commerce.

Pavement Preservation Program - A program that resurfaces or rehabilitates the pavement structure on highways that are built to modern standards to extend the life of the pavement. Included are PPM, Level 2 and 3 highway resurfacing projects, each level describing the intensity of work effort.

Spoon Ditches - Swale type shoulders that are formed to transport highway runoff away from the travel way.

Summer Average Daily Traffic (SADT) - The average traffic on a weekday during the months of July and August.

 Approved by:	
John E. Dority	
Chief Engineer	

This policy shall apply to all Maine Department of Transportation projects except routine maintenance and repair operations and projects on local roads. Such exceptions are governed by their own respective policies. Guardrail and guardrail terminal application shall be based on the highway system designation as set forth in the following sections.

National Highway System

Guardrail

All new guardrail installed on Bridge or Highway projects shall be National Cooperative Highway Research Program *Report* 350 (NCHRP 350) compliant. Guardrail Type 3d- Single Rail shall be used on the interstate and Guardrail Type 3c- Single Rail off the interstate.

All existing guardrail not NCHRP 350 compliant (i.e. guardrail having steel offset brackets) shall be upgraded according to the following conditions:

- On Highway Improvement or Reconstruction projects, guardrail must be upgraded to NCHRP 350 compliance regardless of condition.
- On Pavement Preventive Maintenance (PPM), Level 2 Overlay, and Pavement Rehabilitation projects, NCHRP 230 compliant guardrail needing no work or in need of height adjustment only may stay in place and any extension may be done in kind (i.e. if the existing guardrail is Type 3b then extensions may be done with Type 3b). This shall apply even if there is other guardrail work included in the project scope. Guardrail on these projects shall otherwise be upgraded to NCHRP 350 compliance.
- On Bridge projects which require guardrail replacement or relocation, guardrail must be upgraded to NCHRP 350 compliance.
- On Bridge projects which require no guardrail work, guardrail may stay in place as long as it is NCHRP 230 compliant and is adjusted to the proper height.

Consideration shall always be given to the potential safety and economic benefits of upgrading the guardrail to be NCHRP 350 compliant.

Guardrail Terminals

All new guardrail terminals installed on Bridge or Highway projects shall be NCHRP 350 compliant. The Buried in Backslope and the Flared End terminals are the preferred methods of terminating barrier. The Department currently has three accepted NCHRP 350 compliant flared end terminals: The Fleat 350 (FLared Energy Absorbing Terminal), the SRT 350 (Slotted Rail Terminal), and the Regent. Other NCHRP 350 compliant terminals may be used if they meet the Department's requirements. Each of these systems must be installed with a 1220 mm (4 ft) flare offset. Tangential End terminals shall only be used where adequate vehicle recovery area or adequate space behind the terminal can not be provided. Among others, the ET-2000 and the CAT (Crash Cushion Attenuating Terminal) are NCHRP 350 compliant Tangential End terminals approved by the Department.

All existing guardrail end terminals on Bridge or Highway projects not NCHRP 350 compliant shall be upgraded to NCHRP 350 compliance.

Guardrail

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- On Bridge projects which require no guardrail work, guardrail may stay in place as long as it is NCHRP 230 compliant and is adjusted to the proper height.

Consideration shall always be given to the potential safety and economic benefits of upgrading the guardrail to be NCHRP 350 compliant.

Guardrail Terminals

All new guardrail terminals on Bridge or Highway projects shall be installed according to the following condition:

- If A.A.D.T. is less than 500, terminals may be Low Volume Guardrail Ends.
- If A.A.D.T. is 500 or more, terminals shall be NCHRP 350 compliant.

The Buried in Backslope and the Flared End terminals are the preferred methods of terminating barrier. Tangential End terminals shall only be used where adequate vehicle recovery area or adequate space behind the terminal can not be provided. Acceptable terminals are listed in the National Highway System section above.

All existing guardrail terminals on Bridge or Highway projects shall be upgraded to NCHRP 350 compliance with the following exceptions:

- If A.A.D.T. is less than 500, terminals may be Low Volume Guardrail Ends.
- Existing BCTs on all projects on collector highways with no other guardrail work included in the scope may be left in place if the BCT has not been hit and if there are no crashes recorded at that location.
- All existing MELTs in good condition may be left in place.

If significant repair or replacement is necessary, the replacement shall be NCHRP 350 compliant.

John E. Dority, Chief Engineer Department of Transportation

Page 2 of 2

STATE OF MAINE

Inter-Departmental Memorandum

Date 6/3/93

To Holders of Maine Highway Design Guide **Dept.** Transportation

From Charles Valley, Hwy. Design Engineer Dept. Transportation

Subject Sidewalk Policy – Highway Policy Committee Memo #5 (**REVISED**)

This policy is intended to revise and replace the policy on sidewalks and bikeways issued in 1975 by then Commissioner Roger Mallar. It is intended to address sidewalks only, as a bikeway policy will be developed separately as necessary. The following is the basic policy under which the Department will operate in the foreseeable future, except when CNAQ or Enhancement funds are involved:

- 1. It is the Department's policy to replace existing sidewalks in kind on any reconstruction, rehabilitation or resurfacing project where there is an existing sidewalk. The funding for replacement of the sidewalk in kind will be the same as the funding for the reconstruction, rehabilitation or resurfacing project. This will apply whether the sidewalk is being fully reconstructed or just overlayed with hot bituminous pavement. An exception to the above may be when the Department and Town/City agree in writing to eliminate a sidewalk.
- 2. When a municipality or town requests that sidewalks be constructed as part of a project in an area where there are no existing sidewalks, it would be the municipality's responsibility to pay all of the non-federal share of the cost of the new sidewalk. When the project is not federally funded, it would be the municipality's responsibility to pay 50% of the State's share of the cost. The cost is intended to include gravel, pavement and any additional construction features made necessary by widening for the sidewalk such as retaining walls or barriers. It does not include Right of Way.
- 3. When a municipality or town requests that sidewalks be constructed as part of a project in an areas where there are no existing sidewalks after a project has been advertised, the municipality's responsibility will be the same as in Item 2 except that it will also be responsible for the total cost and acquisition of any right of way necessary to construct the new sidewalk.
- 4. When, as a part of an ongoing project, an existing sidewalk is reconsgructed but the municipality wishes to put down a surface other than that which existed previously, it would then be the municipality's responsibility to pay all of the non-federal share of the additional cost for the requested surface. As an example, a city requests that rather than putting down not bituminous pavement grading "D" that we construct a brick sidewalk...then the non-federal share of the additional cost of the brick surface must be paid for by the town or city involved.

Access Management

The purpose of this policy is to provide guidance to the Regional and Urban/Arterial Program Project Managers when developing plans affecting a rural arterial or major collector where access points currently exist or where new access points are proposed.

Studies show that as the number of access points increase on a roadway, the number of crashes increase and the effective operating speed decreases.

Guiding Principles:

- 1. On non-arterials, Maine DOT's policy prioritizes safety over speed management; speed management policy affects only rural arterials posted at 40 MPH or higher that carry at least 5,000 vehicles per day for the majority of the "corridor". (See the rule/map for more specifics on this issue.)
- 2. The intent of Maine DOT's new driveway/entrance rules is to ensure that all **new** access points meet the appropriate standards.
 - a. Property owner's requesting additional access points should be directed to the Division Office that has jurisdiction over the section of roadway.
 - b. They will be required to fill out an application for a new or modified (see Item 3 below) access point.
 - c. Designers should not use new access points as a bargaining tool in Right of Way negotiations.
 - d. The intent of the new rules is to have all new access points meet the standards to the greatest extent possible.
- 3. The adopted rules have no jurisdiction over any existing access points, unless a change to the intensity of the use or the physical location or grade of the access point is being requested by the land owner.
- 4. The adopted rules may only provide guidance to Maine DOT initiated actions (i.e. reconstruction, rehabilitation, overlays.)
 - a. The designer should look at the number and width of the existing access point(s) serving an individual property. If it will improve safety and allow business activity to continue without disruptions, designers should pursue narrowing any excessively wide (in excess of 42') access point or remove one or more of multiple access points serving the same site. Examples include:
 - i. Businesses with wide open driveways
 - ii. Businesses with more than two access points
 - iii. Residential lots with more than two access points
 - iv. Properties with access points on an intersection radius
 - v. Properties that have a high crash rate

The listed items above are meant as examples and are not meant to be construed as items that must be done on every project. Common sense will go a long way in determining which items should be done and not done. Not

every item is worth using police powers to get done. Not every battle is worth fighting. Hopefully this guidance will help make a given situation better than it is today.

- b. While the rules do not necessarily apply to Maine DOT actions on construction projects, the designer should make an effort to make sensible improvements to unsafe or otherwise non-conforming access (sight distance, width, use of right of way or road for maneuvering onto/off lot, grade entering roadway, proximity to other access points when on a mobility arterial etc.).
- c. In selecting access points to potentially modify, consideration should be given to AADT, number of crashes that have occurred at the specific access in the past 3 years, the horizontal and vertical alignment at the specific access (as relates to posted speed and resulting sight distance) and the nature of the land use served by the access point.
- d. With respect to the land use, consideration should be given to the number of daily/peak hour trips that may be expected at the site as well as the types of vehicles that are likely to enter and exit. Special attention should be given to commercial properties, especially those at intersections.
 - i. Access points should be located a minimum of 75 feet from any intersection, as measured from the intersection lines of the edge of shoulders from both roadways.
 - ii. In no case should an access point be located on the radius located between both roadways.
 - iii. Any changes of access points to meet the corner clearance standard will go a long way toward improving the safety at the intersection.
- 5. Discuss the proposed modification with the property owner. If the proposed improvement generates controversy with the property owner, attempt to negotiate to a mutual position (i.e. how much of the proposed improvement can be supported by the owner?).
- 6. The intent of this guidance is to make mutually agreeable safety improvements without delaying the project's delivery.
 - a. If the advertising date is approaching and agreements haven't been made with the property owners, move forward with the project without those access changes.
 - b. Keep a record (notes for the file) of any attempt to reach agreement.

Any relocation that improves sight distance and any reduction made in the number and/or width of access points will be considered a success!

Exclusive Turnlane/Auxiliary Lane Criteria Table 8-4

Design Element	Design Speed	Traffic Control		Criteria Notes 1,6,7)
Taper Rate Deceleration Length (L _d)	< 40 mph 30 mph 40 mph 50 mph	All (See Note 2)	W x S ² . W x S 120 feet 165 feet 265 feet	
(See Notes 2,3) Storage Length (L _s) (See Note 4)	60 mph	Unsignalized (See Note 4)	370 feet Turning DHV (VPH) <61 61-20 121-180 >180	L _s Minimum Length 100 feet 150 feet >200 feet
		Signalized (See Notes 4,5,6)	number of cars	2.0 times the average that will store in the er cycle during the

Notes:

- 1. <u>Minimum Length.</u> The minimum length of an auxiliary lane will be the taper length (L_t) plus the Storage Length (L_s) .
- 2. <u>Use of Deceleration Length.</u> The designer should consider providing the deceleration length (L_d) , if practical at the following:
 - a. all legs of a signalized intersection (except the truncated leg of signalized T-intersection); and
 - b. the free flowing legs of a stop-controlled intersection for the left-turn lane

Deceleration length need not be considered at stop-controlled legs, nor at the truncated leg of a signalized T-intersection, nor at a right-turn lane for the free-flowing leg at a stop-controlled intersection.

- 3. <u>Measurement of Deceleration Length</u>. As illustrated in figure 8-20, the deceleration length (L_d) also includes the taper length (L_t) . The L_d values in the table assume that the turning vehicle is traveling at a speed of 5 mph below the average running speed before entering the taper.
- 4. <u>Minimum Storage Length.</u> For all intersections where traffic volumes are too low to govern, the minimum length will be 50 feet, (T<11%) or 80 feet (T>10%), where T is the percent of trucks turning.
- Coordination. The Traffic Engineering Division should provide the storage length (L_s) required at signalized intersections.
- 6. <u>Storage Length of Through Traffic</u>. In addition to the table criteria, the length of turning lanes at signalized intersections should exceed the calculated storage length in the through lane adjacent to the turning lane for the design hour.
- 7. English (W = width of the travel lane in feet, S = design or posted speed of roadway in mph).

STATE OF MAINE DEPARTMENT OF TRANSPORTATION

MEMORANDUM

To:	Highway Policy Committee
From:	Stephen Landry, Traffic Engineering
cc:	File
Date:	March 19, 2002
Subject:	Crosswalks

Crosswalks are marked areas where pedestrians can cross a roadway. By law in the State of Maine, any vehicle must yield the right-of-way to a pedestrian who has entered a crosswalk. This law makes it imperative that crosswalk placement, painting and usage be done in a uniform way.

- 1. All crosswalks shall be six (6) feet wide and marked with white paint as shown on the attached sheet.
- 2. All crosswalks shall meet the criteria put forth in the American's with Disabilities Act (ADA).
- 3. All crosswalks should extend from safe landing zone to another. A safe landing zone is an area where a pedestrian is safe from vehicle conflict while waiting to cross or when finished crossing. Islands, walkways and sidewalks are typically considered safe landing zones, while driveways (under normal circumstances) and parking areas are not considered safe landing zones.
- 4. Crosswalks shall be placed in areas where there is sufficient stopping sight distance for the posted speed limit. Crosswalks should be lighted for nighttime use.
- 5. Crosswalks shall have the appropriate signage (W11-2 series from the Manual on Uniform Traffic Control Devices). These signs shall be black symbol on yellow background. There is also an option for black symbol on fluorescent yellow green background. Sign colors should not be mixed in any area.
- 6. Crosswalks should be located a minimum distance of 500 feet apart.
- 7. Crosswalks shall be installed in areas where the speed limit is 35 mph or less.

- 8. No parking shall be allowed within 20 feet of any crosswalk. Signs should be installed indicating that no parking is allowed. (See attachment)
- 9. Prior to installing crosswalks, towns shall enact ordinances dealing with crosswalks. At a minimum, Items 1 through 8 should be included.

Figure 3B-15. Typical Types of Crosswalk Markings

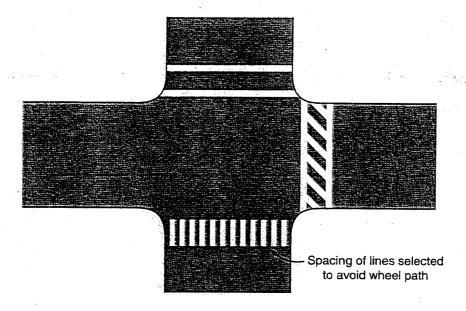


Figure 3B-16. Typical Crosswalk Markings for Exclusive Pedestrian
Phase That Permits Diagonal Crossing

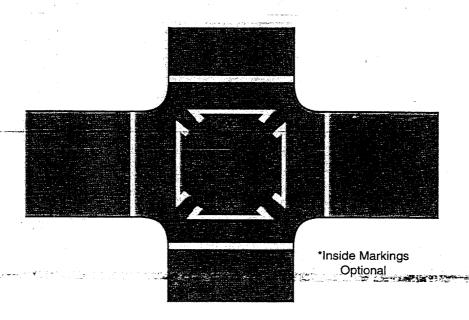
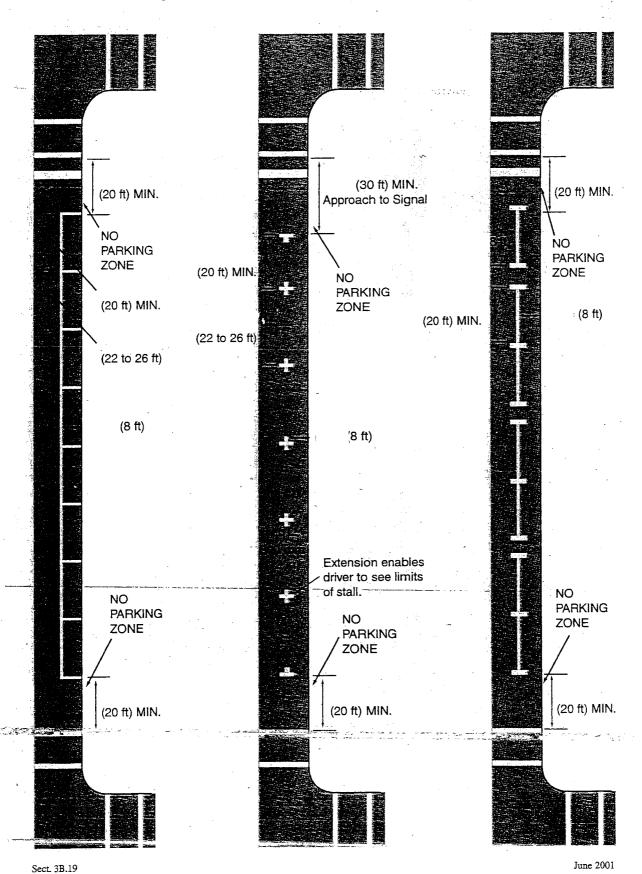


Figure 3B-17. Typical Parking Space Markings



John E. Dority Chief Engineer 16 State House Station Augusta, ME 04333-0016 Phone: (207) 287-2551 Fax: (207) 287-8300

Memorandum

Date:

April 24, 2001

To:

Warren Foster, Carl Croce, Marc Guimont,

Jerry Casey, David Bernhardt, Bruce

Ibarguen

From:

John E. Dority, Chief Engineer

Subject: Flush Traffic Control Island on Capital

Proiects

OR STAMPRO HMA

The use of painted traffic islands at intersections shall be discontinued. If the design of an intersection requires an island or islands, they should be built either of flush concrete or raised curb, preferably granite either edging or vertical curbing depending upon the application.

Other permanent treatments to produce a distinguishable and durable flush island can be used, subject to the approval of Traffic Engineering.

The use of painted islands is being discontinued because of the extremely limited service life of the paint resulting in the loss of definition of the island.

While I strongly support cost saving measures on our projects, positive and lasting traffic islands are essential to the safe operation of at-grade intersections.

When raised islands are installed, highway lighting should be considered to illuminate the entire island. The level of illumination should be sufficient to alert motorists to the presence of this traffic control feature. This is in addition to reflectorized end of island signage.

On light volume roads, the use of reflectorized end of island signage may be considered without illumination.

TED sih

Breakaway Posts

4" x 6" wood post – needs 2 - 1 ½" holes drilled at 4 inches and 18 inches above ground level perpendicular to the traffic. Post should be installed with 6 inch length parallel to the roadway.

6" \times 6" wood post – needs 2 – 2 inch holes drilled at 4 inches and 18 inches above ground level perpendicular to the traffic.

4" x 4" wood post - no modifications needed

U-channel post – must be 2.5 lbs foot. Cannot be doubled up.

U-channel post can hold a sign < 4 square feet

4 x 4 post can hold 4 square feet to 8.99 square feet

4 x 6 post can hold 9 square feet to 15.99 square feet

6 x 6 post can hold > 16 square feet

All signs wider than 60 inches shall be mounted on two wood posts.

Tripod signs:

Old standard: 36 inches to the bottom of the sign.

New Standard: 18 inches or 5 feet.

5 foot standard to be used on the interstate.

Access Management:

New driveways must first get approval from the maintenance division with jurisdiction over the driveway. All driveways must be permitted and checked for sight distance and other standards.

Portable Message Signs:

No non-work related messages. That means patriotic messages, political messages or even "Have a nice day".

Portable Message and add be visible for should be legible for a minimum distance of (650 feet). The

entire message should be able to be read twice at the posted speed.

STATE OF MAINE Maine Department of Transportation Inter-Departmental Memorandum

Date: 12/23/99

To: Marc Guimont

Dept.: Transportation

From: Bruce A. Ibarguen

Dept.: Transportation

Subject: MDOT's Striping Program & Policy

The following is a brief summary of our "current" policy regarding which roads are to be included in our annual striping program.

Centerline Policy

- All routed highways (state & state aid) regardless of AADT receive a centerline every season (yellow-centerline). This covers 5,800 miles of road.
- All other highways (state-summer maintained) receive centerline only if the AADT is greater than 600. This amounts to approximately 1,100 miles of road.

Exception (adopted in 1999): roads with AADT less than 600 which are overlayed are painted once, but are not made a permanent part of the program. We estimate this amounts to 100 miles annually.

• The total mileage of roadway which does not receive a centerline, due to low AADT, amounts to 1,300 miles. However, not all of this highway qualifies for a centerline due to width constraints (less than 18 feet), or condition (unacceptable pavement). Therefore, the total "eligible" mileage considering all factors is about 500 miles.

MDOT is responsible for approximately 8,300 miles of public highway. We stripe a centerline on 7,000 miles every year. There are about 500 more miles which could/should be striped.

Edge line Policy

- State maintained routed highways with an AADT of 2,500 and above receive an edge line every 2 years.
- State maintained routed highways with an AADT of 500-2500 receive an edge line every 3 years.
- Non-routed state highways with an AADT greater than 600, which have received an overlay, and with a pavement width of 28 feet, receive an edge line every 2 years.

Note: Edge line on all "controlled-access" highways is done every year (i.e. Interstate, Portions of Route 1, etc.)

- 1999 Total miles of edge line was 3,123.
- 2000 Total programmed edge line mileage is 3,600.

In 1997, we had \$2,500,000 to complete the striping program. This was increased to \$2,975,000 in 1999. Therefore, we did 750 more miles of roadway edge line, and 1150 more miles of centerline. This included all level 3 construction projects, changing some edge line striping from a 3 year to a 2 year cycle, and almost all of the maintenance overlayed projects.

In 2000, with the addition of the new striper, and the increase in budget to \$3,175,000, we will increase the program again. 500 more miles of edge line, and the 500 miles of roadway (low AADT) identified above will be included in this program. That is, in 2000 we will centerline (yellow) stripe all 7500 miles of eligible highway and all routed highways will begin receiving an edge line on a minimum 2 year cycle.

We allow the Division Engineers to use their discretion in the choice of where pavement markings are placed. (e.g. If a road is to be paved in a following year, then the D. E. may eliminate the edge line striping and apply the mileage to another roadway.)

NO PASSING ZONES

SCHOOL ZONES

The school zone itself, from sign to sign shall be striped no passing.

INTERSECTIONS

Maine state law (section 2070 paragraph 5b) states that you can not pass to the left when approaching within 100 ft of or traversing an intersection or railroad grade crossing, except when turning to the left to enter and intersecting way.

****(right now we stripe no passing for a minimum of 200 ft approaching an intersection. is this enough or should we use sight distance for the speed at intersection?)

NEW ZONES

All complaints shall be looked at by the DTE'S and the decision made by them! Any changes will be diagrammed on paper and sent in to the traffic office so that changes may be made to the master inventory books.

CONSTRUCTION PROJECTS

any striping or stencil changes made because of reconstruction must be looked at byDTE's and changes sent to the traffic office so changes can be made to the inventory books.

PAVEMENT MARKING EXTENSIONS THROUGH INTERSECTIONS

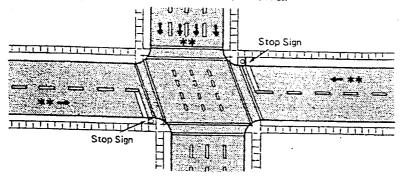
Sections 3b-7 of the mutcd allows for this and the DTE'S can make the decision as to when and where it is needed.

3B-7 Pavement Marking Extensions Through Intersections or Interchanges

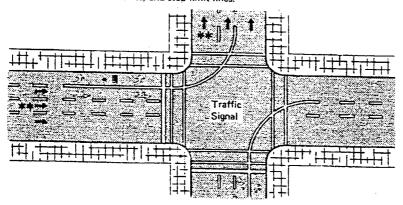
Where road design or reduced visibility conditions make it desirable to provide control or to guide vehicles through an interchange or intersection, (such as at offset, skewed, complexmulti-legged intersections or where multiple turn lanes are used) a dotted line may be used to extend markings as necessary through the interchange or intersection area (figs. 3-9a, 3-11). Where a greater degree of restriction is required, solid lane lines orchannelizinglines may be continued through intersections. A frequent use for the channelizingline is to separate turning movements (figs. 3-9b, 3-9c). SEE NEXT PAGE

Figure 3-9 Typical pavement marking applications.

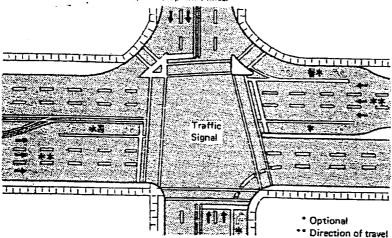
a—Typical pavement marking with offset lane lines continued through the intersection and optional crosswalk lines and stop limit lines.



b-Typical pavement marking with optional double turn lane lines, lane-use turn arrows, crosswalk lines, and stop limit lines.



c—Typical pavement marking with optional turn lane lines, lane use turn arrows, crosswalk lines, and stop limit lines.



BREAKING CENTERLINE AND EDGE LINE FOR INTERSECTIONS

This shall only be done at established intersections, for example: year round maintained roads, state routed highways, state aids, town ways. This does not mean private drives, businesses, or State maintenance lots.

STRIPING STATE AID ROADS THAT DONT MEET ADT

Roads like ones going to the borders, school, or state parks. it seems every year some of these get striped but they are not on our program.

****should they be put on the program? should all of them be done or just some of them?

STRIPING ROADS THAT ARE NOT ON OUR PROGRAM

Only the roads that are on the lists sent to the crew supervisors will be striped. no roads not on the list shall be striped without approval from the traffic office.

EXEMPT RAIL ROAD CROSSINGS

No rail road crossings that are signed exempt will be stenciled.

TRUCK LANES

ending striping for truck lanes.

according to the mutcd the following list of speed limits have the correct distance next to them for the distance from where to end skips to the beginning of the taper.

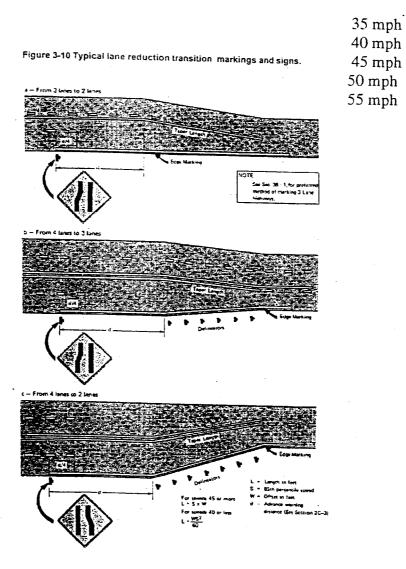
300 ft

356 ft

413 ft

469 ft

525 ft



STRIPING GUIDELINES

State routed highways (outside of cul's)

edge line

2500 adt and above every 2 years

500 adt and above every 3 years

centerline

all every year

state aid (inventory roads)

centerline

600 adt and above every year

paint application

4" line at 15 mils	16 ½ gallons per mile
6" line at 15 mils	24 ½ gallons per mile
4" line at 13 mils	14 1/4 gallons per mile
6" line at 13 mils	21 1/4 gallons per mile

bead application

beads to be applied at the rate of 8 pounds per gallon of paint

new overlay projects

all new overlays will be striped centerline and every effort will be made to put the edge line back on only the section overlaid before the striping season is over.

all lay out of these projects is to be done by the paving technician before any striping will be done. lay out of turning lanes and arrows should be done by thedtes before any pavement markings can be done.

ADDING NEW STENCIL PROJECTS

To add new stencil projects including those added due to construction projects to the pavement marking program, DTE's shall send in a sketch showing what is to be painted and location to the traffic office. (DTE's prior approval from the traffic office to do this is not required.) Once the sketch has been received it will be added to the books and sent on to the pavement marking crew supervisor to be done.

PAVEMENT MARKINGS AT SIGNALS

Any signal inside a compact we do not maintain or do pavement markings at.

Any signal outside a compact that is not a state project we do not maintain or do pavement markings.

Signals that we do not maintain but are outside compacts, we do pavement marking on, unless we have an agreement with the town for them to do the markings.

All signals we maintain we do pavement markings at.

ROUTED HIGHWAYS

EDGELINE

2500 ADT AND ABOVE EVERY 2 YEARS 500 ADT AND ABOVE EVERY 3 YEARS

WIDTH OF 20FT OR LESS NOT TO BE PAINTED

ROUTED HIGHWAYS

CENTER LINE

ALL ROUTED EVERY YEAR

STATE AID

CENTER LINE

600 ADT AND ABOVE EVERY YEAR